

Application Number	Date of Appln	Committee Date	Ward
115139/FO/2017	15th Feb 2017	29 th June 2017	Crumpsall Ward

Proposal Erection of 44 no. 2 and 2.5 storey semi-detached and detached, three and four bedroom dwellings with associated external works, including retaining walls, boundary treatment, car parking, landscaping and associated highway works, including alterations to existing access road - Saddlecote Close, following demolition of existing Sexton's House / Clarksville Farm.

Location Clarksville Farm , Crescent Road, Manchester, M8 5UR

Applicant Mr Abraham Halpern , Breckside Estates LLP, 4 The Cottage, Deva Centre, Manchester, M3 7BE,

Agent Mr DK Seddon, Howard & Seddon ARIBA, 64 Washway Road, M33 7RE.

Description

The application itself relates to a site which lies on the north side of Crescent Road, to the east of Saddlecote Close, and south of the former Clarkesville Farm stables and outbuildings, and the Jewish Cemetery.

The area is predominantly residential in character, with Abraham Moss College to the south of Crescent Road. The site directly to the west has been developed in recent years for 34 semi detached two, three and four bedroomed houses (ref: 097488/FO/2011/N1).

The site rises steeply, and has areas of undulating ground. The site comprises open land (previously used for the grazing of horses), with areas of scrub, and numerous trees. The site has a very distinctive undeveloped character, and is currently enclosed by a brick boundary wall to Crescent Road and a post and wire fence to the private road to the cemetery.

There is a single two storey residential property (the Sexton's house) located within the south of the site, adjacent to Crescent Road and the access road to the cemetery, which would be demolished as part of this proposal . The applicant has indicated that the Sexton would be rehomed within the proposed development.

History

Outline planning permission ref: 097489/FO/2011/N1 for proposed residential development comprising a maximum of 87 residential units. Access and scale to be considered, with all other matters reserved, following demolition of the existing sextons accommodation on Crescent Road.) on land off Crescent Road. This development was allowed on appeal (12/01617/REF) on 4th April 2013.

Planning Application ref: 105294/JO/2014/N1 for the removal of Condition 10 attached to previously approved application ref no. 097489/OO/2011/N1 to enable re location of pond following draining down of the existing pond . Approved 23.05.2014.

Site directly to the west - Planning application ref: 097488/FO/2011/N1- Erection of proposed residential development comprising 34 two storey semi-detached 2, 3 and 4 bed homes with associated works including retaining walls, boundary treatment, car parking, landscaping and associated highway works on land off Crescent Road at junction with Charminster Drive

Planning application ref: 097488/FO/2011/N1 formed part of the site also considered under planning application ref: 097489/FO/2011/N1.

Description of Proposal

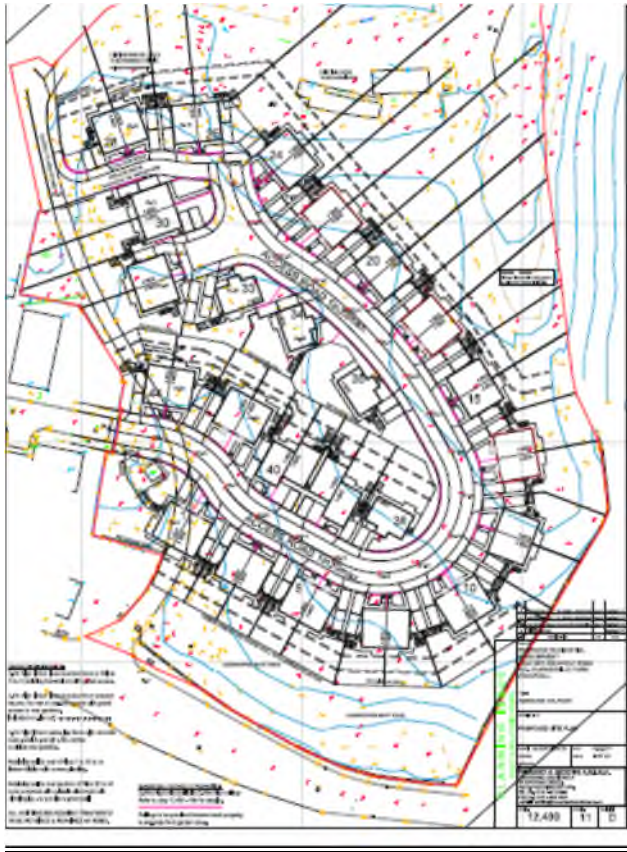
The application seeks approval for the erection of 44 dwellinghouses. The proposal would involve the creation of 20 three bedroom and 24 four bedroomed houses. The houses would be in the form of semi -detached, and detached properties, with front and rear gardens.

The proposed development would necessitate the demolition of the existing Sexton's House on Crescent Road.

In respect of scale, all dwellinghouses would be 2 and 2.5 storeys in height.

Vehicular and pedestrian access would be from Saddlecote Close .





Consultations

The application had been advertised on 2nd March 2017 on site, and in the Manchester Evening News on 21st February 2017.

Local residents/Adjoining occupiers - 6 e-mails were received in relation to the originally submitted plans from residents on Saddlecoate Close .

Concerns expressed include:-

- Access should not be taken from Saddlecote Close, as the road is currently a cul-de-sac used by children to play. More traffic will lead to damage to parked cars, too much disruption. A reflection mirror is needed at the junction of the Charminster Drive and Saddlecote Close;
- The street is narrow enough, with problems for bin wagons and school buses without introducing more traffic. The existing access to the site from Crescent Road should be considered.
- There are already parking problems in the area.
- The scheme will lead to increased litter in the area;
- Overpopulation is attracting the effect of overcrowding especially burglaries. The rear of their house will more accessible and attract burglars.

- The development will rise to noise from a larger population of residents, particularly at night.
- The scheme will lead to the loss of green space , and ruin the local environment
- A resident believes that the location of plot no.44 will lead to privacy issues in relation to his property.

Following the receipt of revised plans a re-notification process has taken place with local residents and businesses. A further e-mail has s been received from the occupiers of 65 Saddlecote Close reiterating their previous concerns

Head of Highway Services -

Has recommended that conditions relating to the submission, implementation and monitoring of a full Travel Plan within 6 months of occupation, a construction management strategy, be attached to any approval .

In additional the following detailed comments have been made:-

ACCESS

Footpath widths have been altered in order to accommodate a 2m provision, in line with MCC recommendations and therefore is accepted by Highways.

It is noted that the access road (Saddlecote Close), as previously stated, is partially unadopted and therefore this section of highway should be picked up by the applicant as part of the S38 Agreement (Highways Act 1980) for the adoption of the wider site. It should be noted that the highway construction will need to be carried out under this agreement to ensure that all elements of new highway infrastructure are constructed to acceptable and adoptable standards. This includes: layout, drainage, street-lighting, surfacing, stats etc.

The gradient of 1 in 20 that has been revised is considered to be in line with adoptable standards and is accepted by Highways.

BOUNDARY TREATMENT

It has been confirmed that the perimeter to each of the properties will be bounded by hedging as opposed to fencing/ railings. This is considered acceptable in principle in ensuring adequate visual permeability through the site to support the safe access/egress of vehicles.

PARKING

All driveway parking spaces have been stated as providing adequate dimensions to accommodate on-site resident parking and therefore accepted by Highways. Parking for the cemetery located adjacent to the site is noted to be retained as existing.

TRIP GENERATION AND JUNCTION CAPACITY

An updated transport statement has been provided. This indicates the level of trips generated from the site as 23 two-way trips during AM peak and 24 movements during the PM peak. This is considered unlikely to give rise to any capacity constraints on the adjacent highway network.

Given the anticipated increase in the level of vehicles associated with the development, it is recommended that the applicant contributes to the junction upgrade of Crescent Road/ Charminster Drive to accommodate pedestrian crossing facilities and tactile paving.

As stated in Design Bulletin 32- Residential Roads and Footpaths 1992, given the number of dwellings proposed, a secondary pedestrian access should be accommodated for the development.

ON-STREET HIGHWAY WORKS

A raised plateau at the hammerhead junction on Saddlecote Close should be implemented in order to act as appropriate traffic calming, as previously stated. This should be agreed via a new Section 278/38 agreement.

The implementation of double yellow lines 10m either side of the S-bend has been acknowledged by the applicant and is considered appropriate for the development in order to prevent inconsiderate on-street parking.

WASTE MANAGEMENT AND SERVICING

A swept path has been provided based on a standard 11.35m refuse vehicle. This is considered acceptable in ensuring that refuse vehicles can access and egress the site whilst remaining in forward gear.

Strategic Lead, Compliance & Community Safety - Recommends that conditions relating to a construction management plan (including construction hours), acoustic insulation of houses from traffic noise from Crescent Road, contaminated land and that the scheme is implanted in accordance with the submitted waste management strategy is attached to any planning approval.

The phase I and II investigation report prepared by Terra Consult dated December 2010 with reference 1355/01 has been examined by appropriate officers . It has been confirmed in the report that there is no risk to controlled waters and the following remediation measures shall be implemented:

1. Gas protection measures in line with a risk level CS2 for those properties within the vicinity of the pond
2. Garden areas for these properties shall be [provided with a clean capping layer of 600mm of clean soil or a venting trench in between the houses and the ponds

However, the following details are required:-

1. The investigation and report was prepared in 2010, therefore an updated site assessment should be provided to confirm that there hasn't been any changes to the land or ground conditions since the initial ground investigation that may lead to contamination and requires additional remediation.

2. A watching brief shall be maintained on site during construction works and any unidentified / unexpected contamination shall be reported to the consultant and the LA immediately
3. After completion of site works, a verification report is required to validate that the work undertaken conforms to the remediation proposals received and agreed. The validation report shall include details of the watching brief.

It is acknowledged that the outcome of the details needed in points 2 and 3 would be provided after any development had been implemented, but confirmation in relation to a watching brief , together with the information outlined in point no.1 is needed at this time.

01/06/17 – Following re-consultation have reiterated the advice outlined above.

Transport for Greater Manchester - Advised that they forwarded their observations to Highways Services for incorporation in Highways Services response.

25/05/17 – Following re-consultation have reiterated the advice outlined above.

United Utilities – Have requested that conditions relating to drainage are attached to any planning approval.

Flood Risk Management Team – Advise that Providing that United Utilities agreed to the proposed connection ,they suggest that drainage conditions are attached to any approval to require that :- 1. No development takes place until surface water drainage works have been implemented in accordance with Non-Statutory Technical Standards for Sustainable Drainage Systems (March 2015) or any subsequent replacements national standards and details that have been submitted to and approved in writing by the Local Planning Authority.

2. No development hereby permitted shall be occupied until details of the implementation, maintenance and management of the sustainable drainage scheme have been submitted to and approved by the local planning authority. The scheme shall be implemented and thereafter managed and maintained in accordance with the approved details.

Strategic Development – Any significant comments will be reported to committee.

Design For Security Team - The proposed development should be designed and constructed in accordance with the recommendations contained within section 3.3 of the submitted Crime Impact Statement dated (16/01/2017 – URN: 2011/0858/CIS/01) and a planning condition should be added to reflect the physical security specification listed within section 4 of the appendices within the submitted Crime Impact Statement.

Greater Manchester Archaeology Unit - There is no archaeological desk based assessment with the application and the Greater Manchester Historic Environment Record has not been consulted. Historic mapping shows Clarkesville as a prominent building on Johnson's map of 1819. Subsequent mapping shows a much larger building than survives today, with large bay window (possibly a mansion), with a another building range at right angles to the east. It is not clear what, if any, of the

historic building fabric survives today. Some of this site falls within the proposed housing area. The site of Clarkesville Farm is a very interesting one topographically as it sits on higher ground overlooking the river valley. This type of site has been shown from archaeological investigations elsewhere to be conducive for early, prehistoric or Romano-British, settlement. GMAAS consider that the proposal site has one known non-designated heritage asset (the farm) and potential for buried archaeological remains. At the moment the archaeological interest, potential and significance is not defined as no archaeological investigations have taken place. GMAAS recommend that archaeological interests are secured through an appropriately worded condition.

Greater Manchester Ecology Unit –As part of the previously approved application here the applicant agreed to resource the creation of a new pond, or improvements to existing ponds, off-site. Now that the pond that was previously on the site has been drained and removed, they have queried whether a similar commitment could be sought as part of this scheme.

Arboricultural Officer- Having visited the site and read the associated documents, and have no objections from an arboricultural prospective to this development. The planting schedule is sufficient to mitigate for the loss of vegetation, it is however recommended that this be conditioned.

Policy

National Planning Policy Framework

The Framework re-iterates that planning law requires that applications for planning permission must be determined in accordance with the development plan unless material considerations indicate otherwise. The statutory status of the development plan remains as the starting point for decision making. However, paragraph 14 states that 'at the heart of the Framework is a presumption in favour of sustainable development' and, in 'decision-taking', this means that development proposals should accord with the development plan should be approved without delay unless:

Any adverse impacts of doing so would significantly and demonstrably outweigh the benefits, when assessed against the policies in this Framework taken as a whole; or
Specific policies in this Framework indicate development should be restricted.

National Policy Framework has been related to the proposed development, with particular emphasis given to the following: These issues have been considered with reference to the core strategy policies as set out in the report.

Core planning principles in Framework - Within the overarching roles that the planning system ought to play, a set of core land-use planning principles should underpin both plan-making and decision-taking. In this case specific weight is given to the need to:

- i. Proactively drive and support sustainable economic development to deliver the homes, business and industrial units, infrastructure and thriving local places that the country needs;

- ii. Secure high quality design and a good standard of amenity for all existing and future occupants of land and buildings;
- iii. Take account of the different roles and character of different areas, promoting the vitality of our main urban areas;
- iv. Support the transition to a low carbon future in a changing climate, taking full account of flood risk and encourage the reuse of existing resources, including conversion of existing buildings, and encourage the use of renewable resources, including renewable energy; Contribute to conserving and enhancing the natural environment and reducing pollution;
- v. Encourage the effective use of land by reusing land that has been previously developed (brownfield land), provided that it is not of high environmental value;
- vi. Promote mixed use developments, and encourage multiple benefits from the use of land in urban and rural areas, recognising that some open land can perform many functions;
- vii. Actively manage patterns of growth to make the fullest possible use of public transport, walking and cycling, and focus significant development in locations which are or can be made sustainable; and
- viii. Take account of and support local strategies to improve health, social and cultural wellbeing for all, and deliver sufficient community and cultural facilities and services to meet local needs.

The following specific policies are considered to be particularly relevant to the proposed development:

- i. Chapter 1: Building a strong, competitive economy - By securing economic growth in order to create jobs and prosperity, building on the country's inherent strengths, and to meeting the twin challenges of global competition and of a low carbon future.
- ii. Chapter 4: Promoting sustainable transport - States that transport policies have an important role to play in facilitating sustainable development but also in contributing to wider sustainability and health objectives.
- iii. Chapter 6: Delivering a wide choice of high quality homes - Refers to the delivery of policies that will result in significant increases to the supply of housing. Policy 6 specifically states that housing applications should be considered in the context of the presumption in favour of sustainable development. Local planning authorities should, subject to a range of specified criteria, seek to deliver a wide choice of high quality homes, widen opportunities for home ownership and create sustainable, inclusive and mixed communities.
- iv. Chapter 7: Requiring good design - Reflects upon the importance of design to the built environment and its contribution to sustainable development and making places better for people. With this in mind, the design of the substantive development has been assessed in relation to the quality and cohesion of its composite building, as well as the function and appearance of public and private spaces.
- v. Chapter 11: Conserving and enhancing the natural environment - Refers to the delivery of policies to minimise pollution and other adverse effects on

the local and natural environment; and encourage the effective use of land by re-using land that has been previously developed (brownfield land) provided that it is not of high environmental value.

National Planning Policy Guidance (NPPG)

The relevant sections of the NPPG are as follows:

Noise states that 'Local planning authorities' should take account of the acoustic environment and in doing so consider:

- whether or not a significant adverse effect is occurring or likely to occur;
- whether or not an adverse effect is occurring or likely to occur; and
- whether or not a good standard of amenity can be achieved.
- Mitigating the noise impacts of a development will depend on the type of development being considered and the character of the proposed location. In general, for noise making developments, there are four broad types of mitigation:
 - engineering: reducing the noise generated at source and/or containing the noise generated;
 - layout: where possible, optimising the distance between the source and noise-sensitive receptors and/or incorporating good design to minimise noise transmission through the use of screening by natural or purpose built barriers, or other buildings;
 - using planning conditions/obligations to restrict activities allowed on the site at certain times and/or specifying permissible noise levels differentiating as appropriate between different times of day, such as evenings and late at night, and;
 - mitigating the impact on areas likely to be affected by noise including through noise insulation when the impact is on a building.
- Design states that where appropriate the following should be considered:
 - layout - the way in which buildings and spaces relate to each other
 - form - the shape of buildings
 - scale - the size of buildings
 - detailing - the important smaller elements of building and spaces
 - materials - what a building is made from

However, many of these issues are also dealt with in detail elsewhere in this report.

Relevant Local Policies Core Strategy

The Core Strategy Development Plan Document 2012 -2027 ("the Core Strategy") was adopted by the City Council on 11th July 2012. It is the key document in Manchester's Local Development Framework. The Core Strategy replaces significant elements of the Unitary Development Plan (UDP) as the document that sets out the long term strategic planning policies for Manchester's future development. Appendix C of the Core Strategy has a list of superseded policies and their replacements .

A number of UDP policies have been saved until replaced by further development plan documents to accompany the Core Strategy. Planning applications in Manchester must be decided in accordance with the Core Strategy, saved UDP policies and other Local Development Documents.

The following specific policies are considered to be particularly relevant to the proposed development:

Policy SP1- Spatial Principles

Policy SP1 advises that the emphasis is on the creation of neighbourhoods of choice, providing high quality and diverse housing around district centres which meet local needs, all in a distinct environment. The majority of new residential development in these neighbourhoods will be in the Inner Areas, defined by the North Manchester, East Manchester and Central Manchester Regeneration Areas. Policy SP1 states under Core Development Principles, that Development in all parts of the City should:-
Make a positive contribution to neighbourhoods of choice including:-

- creating well designed places that enhance or create character;
- making a positive contribution to the health, safety and wellbeing of residents;
- considering the needs of all members of the community regardless of age, gender, disability, sexuality, religion, culture, ethnicity or income;
- protect and enhance the built and natural environment.
- Minimise emissions, ensure efficient use of natural resources and reuse previously developed land wherever possible; and
- Improve access to jobs, services, education and open space by being located to reduce the need to travel and provide good access to sustainable transport provision.

Policy H1 - Overall Housing Provision

Policy H1 of the Publication Core Strategy prioritises residential development on previously developed land, in particular through the re-use of vacant housing or other existing buildings, and policy SP1 which also states that development in all parts of the City should reuse previously developed land wherever possible. These policies are consistent with national and regional policy.

This proposal relates to a windfall site of which is on previously undeveloped greenfield land, and thus could be considered to be unsustainable in relation to developing housing on brownfield sites .

Policy H3 – North Manchester

Policy H3 advises that North Manchester, over the lifetime of the Core Strategy, will accommodate around 20% of new residential development. Priority will be given to family housing and other high value, high quality development where this can be sustained. High density housing will be permitted within or adjacent to the Regional Centre (Strangeways and Collyhurst area), and within Cheetham Hill and Harpurhey district centres as part of mixed-use schemes, as well as along high frequency public transport routes.. The scheme would accord with policy H3 by facilitating the development of larger 3 and 4 bedroomed family accommodation in North Manchester, which currently has a high proportion of 2 bedroomed properties , thus

improving the mix of dwellinghouses available to promote a mixed and diverse community.

Policy H8 - Affordable Housing

The following requirements for affordable housing or an equivalent financial contribution, as set out in Providing For Housing Choice, or any future published SPD and Planning Guidance, currently apply to all residential developments on sites of 0.3 hectares and above or where 15 or more units are proposed. These thresholds will be subject to amendment over the lifetime of the Core Strategy to reflect changing economic circumstances. The targets and thresholds will form part of supporting SPD and/or Planning Guidance.

1. New development will contribute to the City-wide target for 20% of new housing provision to be affordable. Developers are expected to use the 20% target as a starting point for calculating affordable housing provision. It is envisaged that 5% of new housing provision will be social or affordable rented and 15% will be intermediate housing, delivering affordable home ownership options.
2. The proportion of affordable housing units will reflect the type and size of the development as a whole; and where appropriate provision will be made within Section 106 agreements to amend the proportion of affordable housing in light of changed economic conditions, subject to a financial viability assessment.
3. Affordable housing units will be inclusively designed to reflect the character of development on the site.
4. Either an exemption from providing affordable housing, or a lower proportion of affordable housing, a variation in the proportions of socially rented and intermediate housing, or a lower commuted sum, may be permitted where either a financial viability assessment is conducted and demonstrates that it is viable to deliver only a proportion of the affordable housing target of 20%; or where material considerations indicate that intermediate or social rented housing would be inappropriate. In the latter case, such circumstances could include:
 - There is a very high level of affordable housing in the immediate area;
 - There is either a high proportion of social rented (35%), or low house prices in the immediate area compared to average incomes;
 - Affordable housing would be prejudicial to the diversification of the existing housing mix.
 - The inclusion of affordable housing would prejudice the achievement of other important planning or regeneration objectives which are included within existing Strategic Regeneration Frameworks, planning frameworks or other Council approved programmes;
 - It would financially undermine significant development proposals critical to economic growth within the City;
 - The financial impact of the provision of affordable housing, combined with other planning obligations would affect scheme viability;
 - There is a need for additional housing provision for older people or disabled people either as affordable or market housing dependent on the results of a financial viability assessment of the scheme.

5. The Council will also consider the provision of affordable housing which is delivered by taking advantage of other equity based products in addition to those options identified in PPS 3.
6. The definition used for affordable housing is set out in Annex B of PPS 3.

The planning agent has submitted a viability statement for consideration. This matter is dealt with in more detail elsewhere in this report.

Policy EN5 -Strategic Areas for low and zero carbon decentralised energy infrastructure

Within Manchester it is considered that the following strategic areas, indicated on the key diagram, will have a major role to play in achieving an increase in the level of decentralised, low and zero carbon energy supplies available:

- The Regional Centre, which also includes the Oxford Road Corridor and Eastlands District Centres and associated major development sites
- Airport Strategic Site
- Strategic housing location
- Strategic employment locations

Within these areas new development, regeneration and retrofit projects, will be expected to take place in the context of more detailed proposals for decentralised low and zero carbon energy infrastructure in the form of energy proposals plans. The Council will work with all relevant stakeholders, which may include developers, landowners, residents, community groups, private sector partners, utilities companies, neighbouring authorities and other public sector bodies, as appropriate, to bring forward such plans.

Where investment or development is being undertaken into or adjacent to a public building/asset or district heating network, full consideration shall be given to the potential role that these can have in providing an anchor load within a decentralised energy network or in creating opportunities for CO2 reduction funded by contributions.

Policy EN6 - Target Framework for CO2 reductions from low or zero carbon energy supplies

Applications for residential development of 10 or more units and all other development over 1,000 sq m will be expected as a minimum to meet the target shown in Tables 12.1 or 12.2, unless this can be shown not to be viable. This should be demonstrated through an energy statement, submitted as part of the Design and Access Statement. Such a statement will be expected to set out the projected regulated energy demand and associated CO2 emissions for all phases of the development.

Developments smaller than the above threshold, but involving the erection of a building or substantial improvement to an existing building will also be expected to meet the minimum target, where viable, but will not be expected to submit an energy statement.

The target framework relates to three broad development locations and their potential for low and zero carbon, decentralised energy. The areas are defined as follows

Target 1 Network development areas: Locations where the proximity of new and existing buildings, the mix of uses and density of development provide the right conditions to support district heating (and cooling).

Target 2 Electricity intense areas: Locations where the predominant building type has an all electric fit-out such as retail units and leisure complexes.

Target 3 Micro-generation areas: Locations where lower densities and a fragmented mix of uses tend to mean that only building scale solutions are practical.

A Sustainability Statement has been submitted, and this matter is addressed in detail elsewhere in this report.

Policy EN 9- Green Infrastructure

Policy EN9 in the Core Strategy states that new development will be expected to maintain existing green infrastructure in terms of its quantity, quality and multiple function. Where the opportunity arises and in accordance with current Green Infrastructure Strategies the Council will expect developers to enhance the quality and quantity of green infrastructure, improve the performance of its functions and create and improve linkages to and between areas of green infrastructure. Where, in exceptional circumstances, the benefits of a proposed development are considered to outweigh the loss of an existing element of green infrastructure, the developer will be required to demonstrate how this loss will be mitigated in terms of quantity, quality, function and future management .

The Citywide Open Space, Sport and Recreation Study audited and mapped the City's accessible open spaces. However, in Manchester, it is the more finely grained urban green infrastructure that helps give areas and neighbourhoods their distinctive character and also provides many of the linkages in the Green Infrastructure network.

The principle of the redevelopment of this greenfield site for housing has been allowed on appeal. As such whilst the loss of green infrastructure needs to be assessed, in this instance the loss of green infrastructure has been on balance outweighed by the need for residential development outlined in other policies in this report.

Policy EN10- Safeguarding Open Space, Sport and Recreation Facilities

Advises that Council will seek to retain and improve existing open spaces, sport and recreation facilities to the standards identified in the Core Strategy, and provide a network of diverse , multi-functional open spaces.

Proposals will be supported which improve the quality and quantity of accessible open space, sport and recreation in the local area; provide innovative solutions to improving the network of existing open spaces, increase accessibility to green corridors, and enhance biodiversity, and improve access to open space for disabled people. Proposals on existing open spaces and sport and recreational facilities will only be permitted where:

- Equivalent or better replacement open space, sport, or recreational facilities will be provided in the local area; or
- The site has been demonstrated to be surplus for its current open space, sport or recreational function and the City wide standards set out in the Core Strategy are maintained, and
- It could not fulfil other unsatisfied open space, sport or recreational needs, and
- A proposed replacement will remedy a deficiency in another type of open space sport and recreation facility in the local area.

This is private land and as such does not formally constitute open space. The site has been subject to a previous proposal for residential development which was allowed on appeal. There have been no policy changes in relation to open space since the outcome of the appeal.

Policy EN14 -Flood Risk

In line with the risk-based sequential approach contained within PPS25, development should be directed away from sites at the greatest risk of flooding, and towards sites with little or no risk of flooding; this should take account of all sources of flooding identified in the Manchester-Salford-Trafford Strategic Flood Risk Assessment (SFRA).

In addition to the requirements for site-specific Flood Risk Assessments (FRAs) set out in PPS25, an appropriate FRA will also be required for all development proposals, including changes of use, on sites greater than 0.5ha within Critical Drainage Areas (CDAs) and Canal Hazard Zones identified in the SFRA.

All new development should minimise surface water run-off, including through Sustainable Drainage Systems (SUDS) and the appropriate use of Green Infrastructure.

Developers should have regard to the surface water run-off rates in the SFRA User Guide. In CDAs, evidence to justify the surface water run-off approach / rates will be required.

The City of Manchester contains many sections of rivers which are culverted or 'hidden'; where these are indicated in the SFRA beneath the proposed development site, further investigation will be required and the development proposal should take this into account; where feasible and appropriate development should seek to open up culverted/hidden rivers to reduce the associated flood risk and danger of collapse, taking advantage of opportunities to enhance biodiversity and Green Infrastructure. This issue is dealt with in greater detail elsewhere in this report.

Policy EN15 -Biodiversity and Geological Conservation states the Council will seek to maintain or enhance sites of biodiversity and geological value throughout the City. Particular consideration will be given to:

- sites with international or national designations for their biodiversity value. Manchester contains one Site of Special Scientific Interest (SSSI) and other sites of biodiversity value, including 35 Sites of Biological Importance (SBIs)

and 7 Local Nature Reserves (LNRs); and priority habitats found within Manchester, as listed in the Manchester Biodiversity Strategy and included in the Greater Manchester Biodiversity Action Plan (GM BAP);

- protected and priority species, as listed in the Manchester Biodiversity Strategy and included in the Greater Manchester Biodiversity Action Plan (GM BAP);
- sites that are recognised for their geological importance;
- the City Council's objective to protect and conserve the city's existing trees and woodlands and the aim for a net increase in trees across the City.

Developers will be expected to identify and implement reasonable opportunities to enhance, restore or create new biodiversity, either on-site or adjacent to the site, contributing to linkages between valuable or potentially valuable habitat areas where appropriate, with reference to:

- the Manchester Biodiversity Strategy, which provides further details of priority habitats within Manchester and opportunities for biodiversity enhancement including corridors such as the Mersey, Medlock and Irk river valleys, the canals and disused railway lines and areas of managed greenspace particularly in parks;
- the Green Infrastructure Framework for Greater Manchester, which encompasses an ecological framework that seeks to guide and inform habitat creation and repair, including identifying large 'biodiversity opportunity areas' including the Moston Brook Corridor and Nutsford Vale;
- the Open Space, Sport & Recreation Study, which identifies areas where there is a deficit of natural and semi-natural green space, opportunities for green corridors and other linkages;
- Manchester's Climate Change Action Plan;
- the Strategic Flood Risk Assessment (SFRA) for Manchester;
- the Habitats Regulations Assessment (HRA) for Manchester, which gives detailed consideration to Special Areas of Conservation (SACs), including the Rochdale Canal SAC just outside the district boundary;
- the River Basin Management Plan for the North West;
- the Manchester Tree Strategy

Any adverse impacts on biodiversity will need to be justified against the wider benefits of the proposal, assessed against other LDF policies. Where adverse impacts are unavoidable, developers will be required to demonstrate that these will be minimal.

Development should wherever possible seek to maintain, enhance or restore existing geology.

Policy DM1 Development Management - Follows the principles advocated in the aforementioned policies and informs that all development should have regard to the following specific issues for which more detailed guidance may be given within a supplementary planning document:-

- Appropriate siting, layout, scale, form, massing, materials and detail.
- Impact on the surrounding areas in terms of the design, scale and appearance
- of the proposed development. Development should have regard to the

- character of the surrounding area.
- Effects on amenity, including privacy, light, noise, vibration, air quality and
- road safety and traffic generation. This could also include proposals which
- would be sensitive to existing environmental conditions, such as noise.
- Accessibility: buildings and neighbourhoods fully accessible to disabled
- people, access to new development by sustainable transport modes.
- Community safety and crime prevention.
- Design for health.
- Adequacy of internal accommodation and external amenity space.
- Refuse storage and collection.
- Vehicular access and car parking.
- -Effects relating to biodiversity, landscape, archaeological or built heritage.
- -Green Infrastructure including open space, both public and private.
- Flood risk and drainage.
- -Existing or proposed hazardous installations.
- Developers will be required to demonstrate that new development
- incorporates sustainable construction techniques.

For reasons to be outlined below, it is considered the proposal accords with this policy.

National Planning Policy Guidance (NPPG)

The relevant sections of the NPPG are as follows:

Noise states that 'Local planning authorities' should take account of the acoustic environment and in doing so consider:

- whether or not a significant adverse effect is occurring or likely to occur;
- whether or not an adverse effect is occurring or likely to occur; and
- whether or not a good standard of amenity can be achieved.
- Mitigating the noise impacts of a development will depend on the type of development being considered and the character of the proposed location. In general, for noise making developments, there are four broad types of mitigation:
 - engineering: reducing the noise generated at source and/or containing the noise generated;
 - layout: where possible, optimising the distance between the source and noise-sensitive receptors and/or incorporating good design to minimise noise transmission through the use of screening by natural or purpose built barriers, or other buildings;
 - using planning conditions/obligations to restrict activities allowed on the site at certain times and/or specifying permissible noise levels differentiating as appropriate between different times of day, such as evenings and late at night, and;
 - mitigating the impact on areas likely to be affected by noise including through noise insulation when the impact is on a building.
- Design states that where appropriate the following should be considered:
 - layout - the way in which buildings and spaces relate to each other
 - form - the shape of buildings
 - scale - the size of buildings

- detailing - the important smaller elements of building and spaces
- materials - what a building is made from

Saved Unitary Development Plan policies

Saved UDP Policy DC7 states that the Council will negotiate with developers to ensure that new housing is accessible at ground floor level to disabled people, including those who use wheelchairs, wherever this is practicable.

Saved UDP Policy DC16 states that, in considering development proposals for any site, the retention of existing trees and the planting of new trees within the public highway and along the public frontages of the site will be encouraged by the Council. Except where trees are shown to be in poor health or are individually of little amenity value, or where a satisfactory replanting scheme is more desirable, the Council will not normally permit development proposals which would involve the loss of significant trees and would thereby change the visual character of the street.

Supplementary Guidance

The Guide to Development in Manchester Supplementary Planning Document and Planning Guidance was formally adopted in April 2007, and is therefore relevant in this case. This guidance aims to help to develop and enhance a unique and positive sense of place within Manchester and its neighbourhoods. The guidance is grouped under seven key principles recognised by CABI as being those urban design characteristics which create successful places, that is character and context, continuity and enclosure, ease of movement, quality of the public realm, diversity, legibility, and adaptability. The guidance advises that high quality design is a vital attribute of successful neighbourhoods, the design of streets should promote neighbourliness and design should respect and be informed by location. The guidance also advises that buildings should relate well to each other and building heights should be carefully considered. In terms of ease of movement, cul-de-sacs are discouraged, the street network should contribute to the character of the area, safe routes for cyclists and pedestrians will be promoted, and public transport should be easy to use. Places with attractive and successful public areas are encouraged with open spaces reflecting and accommodating future uses with children and young people being considered in the design and location of open spaces. Neighbourhoods should contain a mix of housing, safety and security are vital elements of the urban area, the impact of parking areas should be minimised and new developments should maximise access for disabled people.

Under point 2.12 'Buildings should front onto streets', the Guide states that: It is important to achieve an adequate level of enclosure if streets and open spaces are to provide a sense of place and help people feel comfortable and safe. Buildings should present their main face and pedestrian entrance to the adjacent main street, to contribute to its vitality and interest. Windows and entrances should ensure that the street is overlooked to promote informal surveillance.

Under point 4.15 'Biodiversity', the guide acknowledges policies E2.3 and E2.4 in the UDP which require protection of wildlife and wildlife habitats, and policy E2.6 seeking to prevent the loss of trees. The Guidance advises that developers should highlight how existing species and habitats would be protected and how the proposed

development would enhance the ecological value of the site. It also recommends that mature trees and hedges be retained wherever possible, and that developments should result in no net loss in overall biodiversity value.

The proposed development would accord with the principles outlined above in respect of the provision of strong well defined street frontages, and surveillance within the proposed development. Matters relating to ecology and trees are considered in detail elsewhere in this report.

Providing for Housing Choice - Supplementary Planning Document (SPD) and Guidance (2008)

This outlines the thresholds for the delivery of affordable housing. The SPD seeks affordable housing provision on sites of 15 dwellings or more with a city wide target of 20% affordable housing.

The Guidance identifies the need for family size and other large housing to be reflected in the mix of affordable housing provision. This proposal would provide 3 and 4 bedroom houses suitable for families, as well as smaller accommodated to meet local needs.

North Manchester Strategic Regeneration Framework (NMSRF)

The NMSRF defines the strategic context for the regeneration of North Manchester. It establishes key principles and objectives across the range of inter-related social, economic and physical issues affecting the area. It identifies the importance of the Irk Valley and that a high quality open space network is an important part of the regeneration of the area north of the City Centre.

Housing Strategy policy HG3 Cheetham and Crumpsall, advises that Crumpsall continues to be one of the most desirable and stable areas in North Manchester, and has the potential to influence change in the housing market in surrounding

Manchester Green and Blue Infrastructure Strategy 2015

The Manchester Green and Blue Infrastructure Strategy (G&BIS) sets out objectives for environmental improvements within the City in relation to key objectives for growth and development, and embed Green Infrastructure part of major employment developments. The trees on the site which would be lost as a result of the development contribute to existing tree coverage within the Regional Centre. The development of this site with associated loss of trees was allowed on appeal. The loss of the existing trees is considered necessary in order to deliver residential development. The loss of trees would be compensated by new tree planting on the site (49 replacement trees). The proposal has incorporate mitigation measures to reduce the impacts the proposed development on the green and blue infrastructure in this area of the city, and improve the environment on a route through Crumpsall.

Principle

The principal of development of this land for housing formed part of permission ref: ref: 097489/FO/2011/N1 outline planning permission for proposed residential development comprising a maximum of 87 residential units, following demolition of

the existing sextons accommodation was allowed on appeal (12/01617/REF) on 4th April 2013.

Furthermore, an application to vary the previous permission granted on appeal (ref: 105294/JO/2014/N1) for the removal of Condition 10 attached to previously approved application ref no. 097489/OO/2011/N1 to enable re location of pond following draining down of the existing pond was approved on 23.05.2014.

The site is in a predominantly residential area of Crumpsall. In view of this and that there have not been any significant changes in policy in respect of the site since the outcome of the appeal decision, it is considered that the principle of the site being used for residential purposes has been established and is acceptable in this location.

Access

Vehicular access would be taken from Saddlecote Close, which adjoins the site to the west. Saddlecote Close was formed as part of the previous planning approval ref: 097488. This road layout was approved as part of planning permission reference 097488 for residential development. The road layout for that previous approval was designed to form a vehicular connection via Saddlecote Close to the current application site. A section of Saddlecote Close is not adopted, but the applicant has confirmed that they have a legal right to apply for adoption for this section of highway, and this would form part of an adoption application for the wider development.

The existing sloping ground to Crescent Road would be retained, with levels graded to existing street level. As no access is proposed from Crescent Road the retention of the existing levels in this location would not impact on visibility splays.

Comments received from residents recommending that the existing access from Crescent Road to this site remain have been considered. However, it has been found that it would not be physically possible to implement /retain an access from Crescent Road and achieve a road designed to adoptable standards, as a maximum gradient of 1:20 is required to meet adoption standards. At present the route from Crescent Road to the site has a gradient of approximately 1 in 9. Accordingly access has been proposed to be taken from Saddlecote Close, and the existing access from Crescent Road closed, and would form part of the landscaped frontage of the site.

The scheme has been designed to accommodate two metre wide footways, and the street layout and block form promote pedestrian permeability. The road layout has been designed to incorporate a raised plateau traffic calming feature to reduce vehicle speeds entering this site. Whilst the location has been identified, it is recommended that the finalised design is conditioned.

The front boundary treatment to the proposed houses would be a low wall with railings above, which should enable adequate visual permeability in relation to vehicular movement from driveways.

Siting and Layout

The applicant has incorporated the principles of the Development Guide by ensuring that the majority of dwellinghouses have their principle frontage to the street and car parking within the dwelling curtilage.

The proposal reflects a traditional form of development fronting streets, and incorporates traditional influences in the form of medium density development set around the road pattern. The properties are to be built with a small front garden to provide defensible space from the highway, with a larger private garden to the rear or side for amenity space. Due to the sloping /undulating nature of the existing site, a number of retaining walls would need to be incorporated within rear garden to respond to the changes in levels. It is recommended that a condition be attached to any approval in relation to the design and finish of any retaining structures. The existing sloping ground to Crescent Road would be retained, with levels graded to existing street level.

The indicative levels of car parking accords with guidance. Car parking is provided in-curtilage to the side, or in a limited number of houses, to the front of the house. 100% provision is provided, but in many instances the proposed driveways could accommodate 2 cars. The levels of off street parking provision are considered to be appropriate for the nature of the proposed development.

This approach is considered to be in line with, good urban design principles and the streetscape. All the new properties have off-street parking/ driveways, and the rear/side gardens are a much larger size to the amenity space that the existing houses have in the area. Therefore, it is considered that the layout of this development is acceptable; it reflects the spirit of the existing neighbourhood and is not considered to be overdevelopment of the site.

Density and Scale

The surrounding area is characterised predominantly by two storey residential properties in either terraces or semi-detached form. The level of built form to landscaped areas provides a medium density that is in keeping and an improvement to the layout and density of the vicinity, and the new dwellings are 2 / 2.5 storeys in height to be similar in height/massing to existing properties. It is felt that the application has secured an appropriate scale and massing to development in relation to the surrounding area. Therefore, it is considered that this development is acceptable in relation to density and massing.

In terms of the type and standard of accommodation, policies SP1, H1, H2 and H3 of the Core Strategy seek to ensure that the right type and standard of accommodation is created in the City. This is reiterated within the Residential Design Guide This outlines space standards for new accommodation across the City.

The breakdown of accommodation and sizes within the development is as follows:

The floor area for the proposed 3 bed 4person house is 84.2m.sq, the 4 bed 5person semi-detached is 104.7msq, and the 4 bed 5 person detached property is 104.4sqmsq which accords the minimum gross internal floor area and storage set within the Residential Design Guide.

The mixture of property sizes is considered to be acceptable, particularly as the predominant is four bedroom accommodation. The proposed dwellinghouses also comply with the space standards which is welcomed.

Design and Appearance

This proposal includes the creation of good quality residential accommodation that will improve the appearance of the site and the area. The proposed buildings have a traditional design that utilises the positive features of the site and surrounding properties. The dwellinghouses have been designed as 2 /2.5 storey dwellings to be sympathetic to the existing surrounding buildings and address the street with building frontages and elevational interest. The suggested materials again will be traditional with a mix of brickwork, and grey roof tiles,. All of the houses have lounge windows overlooking the street to increase visual surveillance and to be in line with the City Council's Guide to Development. The houses will be surrounded with quality boundary treatments including brick walls with railings above to the front of properties, with 1.8m high timber fencing to rear gardens to create a suburban appearance.

Therefore, it is considered that the design and external appearance of this development to be acceptable.

Affordability

Policy H8 advises that new development will contribute to the City-wide target for 20% of new housing provision to be affordable, and developers are expected to use the 20% target as a starting point for calculating affordable housing provision. Policy H8 envisages that 5% of new housing provision would be social or affordable rented and 15% would be intermediate housing, delivering affordable home ownership options.

The viability report and the modelling has been assessed by appropriate officers who has confirmed that overall position of the site and the cost of bringing forward a quality housing scheme is borderline viable, which would render it unlikely that an additional affordable housing element could be incorporated in this instance, without compromising the overall viability of bringing forward a successful development.

In relation to the background of this site , the site directly to the west , which formed part of the wider outline planning application (of which this site was a part) allowed on appeal, was developed for 34 socially rented dwellinghouses.

Character of Area and Visual Impact

The site rises steeply, is undulating, grassed and has been used primarily for grazing. The site has trees which are primarily located to the southern end of the site; along the northern boundary and others which are in the north-western corner of the site. The preliminary tree survey schedule accompanying the application indicates that many of the individual trees (as opposed to the groupings) are semi-mature or early mature trees varying between 4 m to 25m in height. In addition there is a hedge which forms much of the eastern boundary. It is considered that the open nature of the site combined with the tree cover, and hedge are a positive contribution to visual amenity and the character of the area.

The proposed retention of the green buffer to Crescent Road would help to maintain the open nature of this section of Crescent Road, and the proposed significant areas of additional shrub and tree planting in this area together with the introduction of a screen planted kribloc retaining wall will soften the visual impact of the proposed rear boundaries of dwellinghouse on the plateau area to the north.

Disabled Access

Externally, level access has been provided to all dwellings and car parking spaces. All internal door widths have been designed to allow a wheelchair user access, provision to enable the potential future installation of a through floor lift has been included, and the stair could be adapted to accommodate a stair lift. Furthermore the first floor bathrooms would comply with lifetime homes standards.

Security

Policy DM1 of the Core Strategy requires that consideration be given to community safety and crime prevention. The planning application is supported by a Crime Impact Statement (CIS), prepared by Design for Security at Greater Manchester Police, which assess the proposal in terms of crime prevention and safety. A Crime Impact Statement has been submitted in support of this application, which demonstrates that prevalent crimes in the area are less serious woundings and criminal damage predominantly against vehicles in the street, which occur at a significantly higher rate than the average rates for North Manchester.

The proposed layout is considered to be acceptable in relation to security, and the housing should be constructed in accordance with the recommendations contained within the submitted Crime Impact Statement, and Secured by Design standards. It is therefore considered appropriate that a condition requiring the development to achieve "Secured by Design" accreditation should be attached to any approval.

Boundary Treatment

Properties would have a low wall with railings above, forming the front boundary with close boarded fencing to form rear boundaries. The applicant has confirmed all existing perimeter boundaries would be retained and repaired as required.

Residential Amenity

It is acknowledged that there are residential properties directly along the western boundary of the site, and there will be a significant increase in the amount of development on the site compared to the current open nature of this site. However, this is a residential proposal in a predominantly residential area. There should not be a significant increase in the noise generated from the site, to warrant refusal of the proposal on the grounds of significant residential disamenity, as the general activity should be limited to residents leaving and returning to the proposed accommodation.

There will be some traffic impacts to existing residents on Saddlecote Road, as this road has effectively been a cul-de-sac, since it was implemented as part of the previous planning approval ref: 097488. However, the road layout for that previous approval was designed to form a vehicular connection via Saddlecote Close to the current application site. It is considered that the introduction of a raised platform close to the entrance to the proposed site would contribute to the reduction of traffic speed both leaving and entering the site. This should reduce the impact of the

increases in the levels of traffic due to the proposed development, on the existing occupiers on Saddlecote Close.

The nearest property to the proposed development would be 17m to the east on Saddlecote Close, and the orientation of the proposed layout would mean the nearest proposed property would be set an angle to the existing dwellings. Furthermore, the height of the new houses is only 2/2.5 storeys, and therefore it is not considered that the development would have a significant impact on any of the surrounding terraced and semi detached properties, and would not have an adverse impact in terms of residential amenity through overshadowing.

Due to the significant changes in levels through the proposed site, it is necessary to incorporate retaining walls in order to facilitate the development. Retaining walls were incorporated into the adjacent development to the west and run parallel to Crescent Road along the rear garden boundary between dwellings on Crescent Road and Saddlecote Close. As part of this scheme, it is proposed to effectively continue the route of the existing western retaining wall extending the length by a further 14m to the east, and then continuing the retaining wall southwards close to the boundary of the westerly most property in the completed development on Crescent Road. This would effectively mean that the remaining 14m of the northern boundary and an 8m length on the western boundary of no.263 Crescent Road would be bounded by high retaining walls. Given the context it is considered that the relationship of houses to the proposed retaining wall is acceptable. It is proposed to condition the finalised design of the retaining walls, as it is indicated on the submitted plans that the southerly retaining wall within the proposed development would be a timber Kribloc retaining wall with screen planting, and it is considered that a similar approach should be taken in relation to the western boundary. It is considered that a well designed retaining wall should not adversely impact on the existing occupiers in relation to visual amenity. Also in relation to the occupiers of no.263 Crescent Road, the two proposed retaining walls are located to the north and east, as such any loss of light should not be so significant to warrant refusal of planning permission.

With regard to the proposed uses on the site and potential noise, it is considered that the buildings could themselves be suitably insulated to prevent occupiers of the dwellings being disturbed from external noise sources.

Waste Management Strategy

The applicant has submitted a waste management strategy in support of the application. Each dwellinghouse would have 4no. bins in the rear garden (general waste /garden waste//paper/tin & glass), with each property owner /occupier being responsible to place the respective reuse bins at the back of pavement on collection day. This has been assessed and agreed by the Strategic Lead, Compliance & Community Safety officers and highways officers. A condition has been included to ensure that waste will be dealt with in line with the approved strategy.

Landscaping /Trees

The applicant's intention is to re-grade significant areas of the site in order to accommodate the proposed houses.

The applicant has provided plans which show that four trees on the northern boundary would be retained, together with other trees on the eastern and southern

boundaries. The scheme would require the removal of 54 trees to facilitate the proposed housing development. Due to the changes in levels on this site, it is not possible to accommodate significant levels of replacement tree planting, as part of the proposed residential development. However, 49 trees would be planted to mitigate against the loss of trees. These would be located in front garden areas, with a significant proportion of the proposed tree planting being located in the landscaped buffer alongside the boundary to Crescent Road.

Front and rear garden areas would be grassed, with some properties having hedging along boundaries between parking spaces, other dwellinghouses having feature shrubs in front garden areas. The rear gardens would be stepped to suit existing ground levels using railway sleepers or similar.

It is recommended that a landscaping condition to be attached to any planning approval to ensure the implementation of the approved scheme and to ensure that any planting which dies or is removed in 5 years is replaced.

Ecology

An ecological assessment accompanied this application. Boggart Clough Local Nature Reserve lies 1000m to the north-east of the site, and the Reservoirs at Harpurhey are a Site of Biological interest are approximately 500m to the west.

The assessment identified that four records of bat roosts have been found in the area of search, and 19 records of the presence of bats in the area. However, no evidence of bats roosts were found, and the site was assessed as having a low suitability for commuting and foraging bats. The trees and scrub on the site are suitable for nesting birds. The site is therefore considered to be of local value to birds. It is therefore recommended that this aspect of the scheme is conditioned, and that a condition is attached to require that works are not undertaken during the bird nesting season.

The site comprises mainly improved grassland with scrub and occasional trees around the boundary. However, there are also stands of Japanese Knotweed. In addition to treating the knotweed within the site, consideration would need to be given to the prevention of the spread of Japanese Knotweed from the adjacent land to the east.

The comments from the Greater Manchester Ecology Unit have been noted, however, there has been no pond on the site for a number of years, since the pond was drained down approximately 5 years ago. It is therefore considered to be unreasonable to require further mitigation works in regard to the former pond.

Open Space

Local open space within 500m of the site, which include Crumpsall Park, Abraham Moss Centre playing fields, Waterloo Street playing fields, Hoddlesdon Street Allotments, and two areas of natural and semi natural open space in the Irk Valley.

Although the site is not referred to in the City Council's Open Space Assessment, or the Irk Valley Local Plan it is considered that the value that the site contributes to visual amenity, due to the open nature of this part of the site and the amenity it offers at this location, forming a green buffer between built up areas is important locally.

The proposal would retain and enhance the green frontage to Crescent Road, thereby retaining an element of openness in the streetscene, and reducing the visual impact of the proposed dwellinghouses to be located on the plateau area directly to the north.

Lighting

At present no external lighting is indicated (other than street lighting). However, due to the need to achieve Secure by Design Accreditation, it is recommended that a condition be attached to any approval to require full details of any external lighting to be erected within the development to be submitted and agreed in writing. This is in order to enable the full impact of such proposals on security, and bats, should external lighting be required.

Contaminated Land

The site is currently comprises former farm buildings, a Sexton's house, and areas of grassland with trees. The phase I and II investigation report has been examined, and it has been confirmed that there is no risk to controlled waters; that gas protection measures in line with a risk level CS2 for those properties within the vicinity of the former pond are required and the garden areas for those properties should be provided with a clean capping layer of 600mm of clean soil or a venting trench in between the houses and the ponds.

In addition an updated site assessment should be provided to confirm that there hasn't been any changes to the land or ground conditions since the initial ground investigation in 2010 that may lead to contamination and requires additional remediation. A watching brief and a validation report will also be needed. In view of the above requirements it is therefore recommended that a condition be attached relating to contaminated land and landfill gas measures be attached to any approval pending the receipt of the information outlined above.

Flood Risk

The application site is located in flood zone 1 'low probability of flooding'. However, the site lies within a critical drainage area (an area where there are complex surface water flooding problems from ordinary watercourses, culverts and flooding from the sewer network). These areas are particularly sensitive to an increase in rate of surface water run off and/or volume from new developments which may exasperate local flooding problems. As such, policy EN14 states that developments should seek to minimise the impact on surface water run off in a critical drainage area.

The applicant has prepared a drainage scheme in support of their planning application. This has been considered by the City Council's flood risk management. On the basis of the observations received it is recommended that conditions relating to a surface water drainage and maintenance scheme are attached to any approval.

Sustainability issues

A Sustainability Statement has been submitted to accompany this application, which confirms that the development will comply with Part L of the Building Regulations, and identifies measures in respect of the building fabric /thermal performance, predicted levels of carbon emissions, passive control measures, and necessary

provisions for energy efficiency. It is however recommended that a condition be attached to any approval to ensure the development will achieve a sustainability rating based on the principles of the Building Research Establishment Environmental Assessment Method (BREEAM) rating of at least three star sustainability rating under the code for sustainable homes, including the requirement for the submission of post construction review statement and/or certificate.

Archaeology

Historic maps shows Clarkesville as a prominent building on Johnson's map of 1819. Subsequent mapping shows a much larger building than survives today, with another building range at right angles to the east. Some of this site falls within the proposed housing area. Observations by the Greater Manchester Archaeological Advisory Service regarding the site of Clarkesville Farm being a type of site which has been shown from archaeological investigations elsewhere to be conducive for early, prehistoric or Romano-British, settlement have been noted. In view that GMAAS consider that the proposal site has one known non-designated heritage asset (the farm) and potential for buried archaeological remains it is recommended that archaeological interests are secured through an appropriately worded condition.

Traffic Generation

A Transport Statement has been submitted to accompany the application. The report concludes that the site is well served by existing transport infrastructure, and is accessible to a range of key services and facilities. Furthermore, that the site can be safely and appropriately accessed, and that traffic generated by the proposal would not have a detrimental impact on the operation of surrounding transport networks.

The site benefits from good accessibility by public transport, lying in close proximity to regular bus services on Crescent Road, and metrolink services at Abraham Moss tram stop. There are however, no dedicated on-road cycle facilities in proximity to the site. The levels of accessible public transport will assist in reducing any traffic impacts. Furthermore given the observations by Highways Services, it is recommended that the development, submission, implementation and monitoring of a full Travel Plan within 6 months of occupation be attached as a condition of any planning consent in order to identify and promote the use of sustainable modes in the area.

Given the potential for traffic movements to be restricted by on street indiscriminate parking, particularly at the 'S' bend located towards the eastern end of Saddlecote Close, it is recommended that a condition requiring detailed designs (including specifications) of all highways works (including the requirement of any Traffic Regulation Orders) attached to any approval.

Based on existing traffic movements within the area and models which project traffic generation and distribution, the proposed development is considered to be acceptable and the traffic which is generated can be accommodated within the existing highway network.

On balance, it is considered that the site would not generate detrimental level of traffic that would impact on the operation and safety of the local highway network,

and that there are no transport related issues which would preclude this proposed development, subject to the suggested traffic conditions outlined above.

Cycle storage

The applicant has submitted details of a shed to be located within the rear curtilage of each property with a secure loop to accommodate cycle storage. As such 44 cycle storage spaces (minimum) are proposed. This is considered to be a satisfactory approach, and a condition is recommended to ensure the sheds are installed prior to occupation of the dwellings.

Construction Management Plan

It is recommended that a construction management plan be provided by the developer prior to construction works beginning detailing the proposed construction/fit-out arrangements particularly in relation to anticipated vehicular movements together with any associated proposals for delivery, storage and disposal / collection of materials, and that this aspect of the scheme is conditioned.

Other Comments made by residents

Traffic and on street parking - on the indicative layout all properties have been provided with driveways, many of which would be able to accommodate two cars. It is therefore unlikely that this proposal would lead to significant levels of on-street parking on Saddlecote Close if the principles of the layout submitted are followed.

Conclusion

The site is relatively unique in the locality. Its form and function as a green buffer between housing areas, has value in visual and amenity terms for the character of the area. The redevelopment of this site with a quality development, retaining the green buffer to Crescent Road would bring forward major environmental and regeneration benefits is welcome.

Human Rights Act 1998 considerations – This application needs to be considered against the provisions of the Human Rights Act 1998. Under Article 6, the applicants (and those third parties, including local residents, who have made representations) have the right to a fair hearing and to this end the Committee must give full consideration to their comments.

Protocol 1 Article 1, and Article 8 where appropriate, confer(s) a right of respect for a person's home, other land and business assets. In taking account of all material considerations, including Council policy as set out in the Core Strategy and saved policies of the Unitary Development Plan, the Head of Planning, Building Control & Licensing has concluded that some rights conferred by these articles on the applicant(s)/objector(s)/resident(s) and other occupiers and owners of nearby land that might be affected may be interfered with but that that interference is in accordance with the law and justified by being in the public interest and on the basis of the planning merits of the development proposal. She believes that any restriction on these rights posed by the approval of the application is proportionate to the wider benefits of approval and that such a decision falls within the margin of discretion afforded to the Council under the Town and Country Planning Acts.

Recommendation APPROVE

Article 35 Declaration

The local planning authority in making its decision has had due regard to paragraph 187 of the National Planning Policy Framework as well as the development plan, national planning policy and other material considerations and recommends approval the application for the reasons outlined in this report. Officers have worked with the applicant in a positive and proactive manner based on seeking solutions to problems arising in relation to dealing with highway and boundary treatment matters with the applicant to negotiate a revised scheme to create a more appropriate design. Therefore, the scheme is now acceptable and in accordance with the Development Plan.

Conditions to be attached to the decision

1) The development must be begun not later than the expiration of three years beginning with the date of this permission.

Reason - Required to be imposed pursuant to Section 91 of the Town and Country Planning Act 1990.

2) The development hereby approved shall be carried out in accordance with the drawings numbered :-

J4284 EX01 (Drainage Strategy)solely in relation to drainage; 12490 105 rev. A (site edged red); J4284 02 (existing and proposed site levels) , date stamped as received on 26th January 2017

12490 05 rev.H (sketch site plan) as amended by 12490 10 rev.D (site plan indicating ownership boundary) which shows the updated site layout;
12490 06 rev.H (Floor plans)
date stamped as received on 13th February 2017;
SCP/16192/ATR01 (swept path analysis)
Typical Garden Shed.

Design and Access Statement; Economic Viability Assessment; Viability Appendices ; Proposed drainage strategy;; Waste Management Strategy; Crime Impact Statement Version A:16/01/17 ref:2011/0858/CIS/01; Tree Survey and plan date stamped as received on 13th February 2017; Sustainability Statement; Ecological Appraisal by Urban Green dated October 2016;

12490 09 rev. B (site sections) date stamped as received on 15th February 2017;

J4284 01 rev.A (Proposed retaining wall layout) J4284 03 (proposed sections) date stamped as received on 5th May 2017;

12490 10 rev.D (site plan indicating ownership boundary); 12490 12 rev.A (plot 29 & 30 plans and elevations) date stamped as received on 10th May 2017;

Phase 1 & 2 Site investigation report 2010 analysis by TerraConsult dated 15th May 2017;

Transport Statement (ref: TH/162/TS/0) dated May 2017 received on 4th May 2017

E-mail received 13th June 2017 from the planning agent in relation to land ownership

Email from applicant Mr Halpern dated 15th June 2017 confirming he has a legal easement right over the adopted part of Saddlecote close with the ability to apply for road adoption on this section so it joins his site.

PR/17/ AH01/GA/01 rev D (landscape works); 12490 11 rev.D (Proposed site Plan); 12490 13 rev. A (front boundary walls), 12490 08 rev.F (elevations and floor plans plots 33-36); received on 15th June 2017 ;

Reason - To ensure that the development is carried out in accordance with the approved plans, pursuant to policies SP1 and DM1 of the Core Strategy for Manchester.

3) Prior to commencement of works on site, a construction method statement (including details of any vibro compaction works), hours of operation, and a strategy for the management of construction traffic, (including information regarding site approach routes, and directional signage) shall be submitted to and approved in writing by the City Council as local planning authority. The construction method statement and strategy shall be implemented in full throughout the construction period.

Reason - To safeguard the amenities of nearby residents, pursuant to policies SP1 and DM1 of the Core Strategy for Manchester.

4) No above ground works forming the development that is hereby approved shall commence unless and until samples and specifications of all materials to be used on all external elevations of the development have been submitted to and approved in writing by the City Council as local planning authority.

Reason - To ensure that the appearance of the development is acceptable to the City Council as local planning authority in the interests of the visual amenity of the area within which the site is located, as specified in policies SP1 and DM1 of the Core Strategy.

5)The car parking shall be surfaced in permeable materials, demarcated and made available for use before the development is occupied, in accordance with details to be submitted to and approved in writing by the City Council as Local Planning Authority. The car parking spaces shall then be available at all times whilst the buildings are occupied.

Reason - To ensure that there is adequate car parking for the development proposed when the buildings are occupied, pursuant to saved policy E3.3 of the Unitary Development Plan for the City of Manchester and policies SP1 and DM1 of the Core Strategy for Manchester.

6) The development shall be implemented in accordance with the recommendations within the submitted Crime Impact Statement (16/01/2017 – URN:2011/0858/CIS/01), including the physical security specifications listed within section 4 of the appendices within the submitted Crime Impact Statement. The development shall only be carried out in accordance with these approved details. The development hereby approved shall not be occupied or used until the Council as local planning authority has acknowledged in writing that it has received written confirmation of a Secure by Design accreditation.

Reason - To reduce the risk of crime pursuant to policies SP1 and DM1 of the Core Strategy for Manchester and to reflect the guidance in the National Planning Policy Framework.

7) Prior to the commencement of the development hereby approved, an Environmental Standards Statement outlining how the development will achieve a sustainability rating based on the principles of the Building Research Establishment Environmental Assessment Method (BREEAM) rating of at least three star sustainability rating under the code for sustainable homes shall be submitted and approved by the City Council as the Local Planning Authority. A post construction review statement and/or certificate shall be submitted to and approved in writing by the City Council as local planning authority before any of the buildings hereby approved are first occupied.

Reason - In order to minimise the environmental impact of the development pursuant to policies SP1, EN4-EN7 and DM1 of the Core Strategy for the City of Manchester and the principles contained within The Guide to Development in Manchester SPD (2007) and the National Planning Policy Framework.

8) Full detailed designs (including specifications) of all highway works (including any requirements for Traffic Regulation Orders, and off site highway works) shall be submitted to and approved in writing by the City Council as Local Planning Authority, prior to the development commencing. The development shall be implemented in accordance with the approved details prior to any residential unit within the development being first occupied.

Reason - In the interests of highway safety, pursuant to policy T1 of the Core Strategy for Manchester.

9) Prior to the installation of the raised plateau to the entrance of the site finalised detailed designs of the raised plateau, together with the location, shall be submitted to and approved in writing by the local planning authority. The scheme shall subsequently be implemented in accordance with the approved details before the development is completed.

Reason - In the interests of highway safety and pedestrian safety, pursuant to policies DM1, SP1 and T1 of the Core Strategy for Manchester.

10) Within 3 months of commencement of development hereby approved details of a Travel Plan with the objective of reducing car borne journeys; and including particulars of its implementation and monitoring of effectiveness and how measures

to improve effectiveness shall be implemented , (with specific reference to traffic management measures based on monitoring data of the existing traffic volumes in conjunction with the future traffic forecasts) for that phase of development shall be submitted to and approved in writing by the City Council as local planning authority prior to first occupation of any premises within the Phase ; For so long as the use is in operation the local planning authority shall be provided with such information as specified in the Travel Plan and all measures that are identified that can improve the effectiveness of the Travel Plan shall be adopted and implemented within a timescale agreed in writing by the local planning authority, as part of the Travel Plan.

Reason - To assist promoting the use of sustainable forms of travel pursuant to policies SP1, T2 and DM1 of the Core Strategy for Manchester.

11) Within six months of first occupation of the development, a repaving strategy for the public footpaths and redundant vehicular crossings shall be submitted to and approved in writing by the City Council as local planning authority. The approved scheme shall be carried out in accordance with an implementation programme to be approved by the Local Planning Authority.

Reason - In the interests of amenity and to ensure that paving materials are consistent with the use of these areas as pedestrian routes, pursuant policy DM1 of the Core Strategy for Manchester.

12) The development shall not be occupied until the gardens sheds incorporating secure loop brackets for bicycle storage have been provided in accordance with details submitted to the City Council as local planning authority on 15th February 2017. These facilities shall then be permanently retained for bicycle storage.

Reason - To ensure that adequate provision is made for bicycle storage so that persons occupying or visiting the development have a range of options in relation to mode of transport, pursuant T1 and DM1 of the Core Strategy for Manchester.

13) Details of access proposals to the development for mobility impaired persons including route widths, levels and gradients, shall be submitted to and approved in writing by the City Council as local planning authority before development of each Phase commences. The approved details shall be implemented before the development is first brought into use.

Reason - To ensure that satisfactory disabled access is provided by reference to the provisions of the Unitary Development Plan for the City of Manchester saved policy DC7, and policy DM1 of the Core Strategy for Manchester.

14) Prior to the erection of the above ground works, an acoustic attenuation scheme to protect against noise from Crescent Road including particulars of the internal noise levels to be achieved, acoustic glazing and any acoustically treated ventilation to be installed as part of that Phase of the development , shall be submitted to and approved in writing by the City Council as local planning authority. There may be other actual or potential sources of noise which require consideration on or near the site, including any local commercial/industrial premises. The approved noise insulation scheme shall be completed before any of the dwelling units are occupied.

The proposed site is in close proximity to North Manchester General Hospital so an assessment of LAmax levels should also be made taking into account noise from siren's. Noise survey data must include measurements taken during a rush-hour period and night time to determine the appropriate sound insulation measures necessary.

Reason - To protect the amenity of the occupants of the premises once the development hereby approved is occupied, pursuant policies SP1, H1 and DM1 of the Core Strategy for Manchester.

15) a) Before each Phase of the development hereby approved commences, a report (the Preliminary Risk Assessment the Desk Study) to identify and evaluate all potential sources and impacts of any ground contamination, groundwater contamination and/or ground gas within that Phase shall be submitted to and approved in writing by the City Council as local planning authority. The Preliminary Risk Assessment shall conform to the City Council's current guidance document (Planning Guidance in Relation to Ground Contamination).

The Desk Study shall be prepared by a suitably qualified person whose competence to carry out the Desk Study has been approved in writing by the City Council as local planning authority (an "Expert").

In the event of the Preliminary Risk Assessment Desk Study identifying risks which in the written opinion of the Local Planning Authority require further investigation, the development of that Phase shall not commence until a scheme for the investigation of the site and the identification of remediation measures (the Site Investigation Proposal Scheme) has been submitted to and approved in writing by the City Council as local planning authority.

The measures for investigating the site identified in the Site Investigation Proposal Scheme shall be carried out, by an Expert (being an identified individual whom the Council as LPA has agreed in writing is sufficiently qualified to act as Expert) before development of that Phase commences and the Expert shall prepare a report outlining what measures, if any, are required to remediate the land (the Site Investigation Report and/or Remediation Strategy) which shall be submitted to and approved in writing by the City Council as local planning authority.

b) When the development of that Phase commences, the development shall be carried out in accordance with the previously agreed Remediation Strategy / Site Investigation Report, and a Completion/Verification Report assessing compliance with the Remediation Strategy and/or Site Investigation Report shall be submitted to and approved in writing by the City Council as local planning authority.

In the event that ground contamination, groundwater contamination and/or ground gas, not previously identified, are found to be present on the site at any time before the development of the individual Part Phase shall cease and/or the development shall not be occupied until, an Expert has prepared a report outlining what measures, if any, are required to remediate the land (the Revised Remediation Strategy / Site Investigation Report) which shall be submitted to and approved in writing by the City Council as local planning authority and the development shall be

carried out in accordance with the Revised Remediation Strategy / Site Investigation Report, which shall take precedence over any Remediation Strategy Site / Site Investigation Report or earlier Revised Remediation Strategy / Site Investigation Report.

Reason - To ensure that the presence of or the potential for any contaminated land and/or groundwater is detected and appropriate remedial action is taken in the interests of public safety, pursuant to policies EN18 and DM1 of the Core Strategy for Manchester.

16) The development hereby approved shall be implemented in full accordance with the Waste Management Strategy submitted to the Local Planning Authority, which includes a scheme for the disposal of refuse including segregated waste for recycling. The details of this approved scheme shall be implemented as part of the development and shall remain in situ whilst the use or development is in operation.

Reason - In the interests of the amenities of the occupiers nearby properties in order to comply with policies SP1 and DM1 of the Core Strategy for Manchester.

17) The development shall be implemented in accordance with the detailed cross section plans of the existing and proposed external ground levels shown on plan refs: 12490 09 rev. B (site sections) and J4284 03 (proposed sections).

Reason - To safeguard residential and visual amenity, and to ensure that a satisfactory development is carried out, pursuant to policy DM1 of the Core Strategy for Manchester .

18) Prior to the installation of the retaining walls on the site, finalised detailed elevational designs of the retaining , shall be submitted to and approved in writing by the local planning authority. The scheme shall subsequently be implemented in accordance with the approved details before the development is completed.

Reason - In the interests of visual amenity , pursuant to policies DM1, and SP1 of the Core Strategy for Manchester.

19) Prior to the commencement of development, details of the strategy for all external lighting, including lighting on the buildings, within a building's curtilage, and lighting units within the public realm works, shall be submitted to, and approved in writing by, the City Council as local planning authority. The development shall be implemented in accordance with the approved details.

Reason - To ensure adequate lighting within the development and to ensure full accessibility within the public realm works, pursuant to policies SP1 and policy DM1 of the Core Strategy for Manchester.

20) If, when the lighting units are illuminated, they cause glare or light spillage which is in the opinion of the City Council as Local Planning Authority to the detriment of adjoining and nearby residential properties, such measures as the Council as LPA confirm in writing that they consider necessary including baffles and/or cut-offs shall be installed on the units and adjustments shall be made to the angle of the lighting

units and the direction of illumination, which shall thereafter be retained in accordance with details which have received the prior written approval of the Local Planning Authority.

Reason - In order to minimise the impact of the illumination of the lights on the occupiers of existing and proposed nearby residential accommodation, pursuant to policies SP1 and policy DM1 of the Core Strategy for Manchester.

21) The landscaping scheme shall be implemented in accordance with the details shown on plan ref: PR/17/ AH01/GA/01 Rev.D (landscape works) . The approved scheme shall be implemented not later than 12 months from the date of first occupation of any building within that Phase. If within a period of 5 years from the date of the planting of any tree or shrub, that tree or shrub, or any tree or shrub planted in replacement for it, is removed, uprooted or destroyed or dies, or becomes, in the opinion of the local planning authority, seriously damaged or defective, another tree or shrub of the same species and size as that originally planted shall be planted at the same place.

Reason - To ensure that a satisfactory landscaping scheme for each Phase of the development is carried out, that respects the character and visual amenities of the area, in accordance with policies SP1, EN9 and DM1 of the Core Strategy for Manchester.

22) No trees shall be felled, or have any works undertaken on them, during the bird nesting season (March to August inclusive).

Reason - To ensure the protection of wildlife habitats in the locality, pursuant to policy EN15 of the Core Strategy for Manchester.

23) Within the development, all trees, shrubs and hedges within the development and/or trees whose root structure may extend within the development, which are to be retained shall be fenced off in accordance with British Standard 5837 (1991) before any building or other operation approved by this permission is carried out . Thereafter, no excavation or other building or engineering operations shall take place and no plant, machinery or materials (including excavated material) shall be placed, deposited, stored or stacked within any such fence during the construction period.

Reason - In order to avoid damage to trees/shrubs adjacent to and within the site which are of important amenity value to the area, pursuant to policy EN9 of the Core Strategy for Manchester .

24) In this condition "retained tree" means an existing tree (including street trees), shown as being retained as part of the landscaping scheme to be approved as part of a reserved matters application; and paragraphs (a) and (b) below shall have effect until the expiration of 5 years from the date of the occupation of the buildings for its permitted use.

(a) No retained tree shall be cut down, uprooted or destroyed, nor shall any retained tree be topped or lopped other than in accordance with the landscaping scheme to be approved at reserved matters stage without the written approval of the local planning authority. Any topping or lopping

approved shall be carried out in accordance with British Standard 5387 (Trees in relation to construction).

(b) If any retained tree is removed, uprooted or destroyed or dies, another tree shall be planted at the same place and that tree shall be of such size and species, and shall be planted at such time, as may be specified in writing by the local planning authority.

(c) The erection of fencing for the protection of any retained tree along the eastern boundary shall be undertaken in accordance with the approved plans and particulars before any equipment, machinery or materials are brought on to the site for the purposes of the development, and shall be maintained until all equipment, machinery and surplus materials have been removed from the site. Nothing shall be stored or placed in any area fenced in accordance with this condition and the ground levels within those areas shall not be altered, nor shall any excavation be made, without the written consent of the local planning authority.

Reason - In order avoid damage to trees/shrubs adjacent to and within the site which are of important amenity value to the area and in order to protect the character of the area, in accordance with policies EN9 and EN15 of the Core Strategy.

25) No part of the development hereby permitted (such works to include the demolition of built structures) shall commence until, details of a permanent bat roost, as part of the approved development and associated lighting scheme have been submitted to and agreed in writing with the Local Planning Authority. The permanent bat roost and lighting scheme should then be installed in accordance with the agreed design.

Reason - To ensure the protection of wildlife habitats in the locality, pursuant to policy EN15 of the Core Strategy for Manchester.

26) Before the erection of the above ground floor structures, full details of a scheme for the management, destruction and /or disposal of Japanese Knotweed, to be carried out by the developer, shall be submitted to and approved in writing by the City Council as Local Planning Authority. This plan shall include a timetable for implementation. Should a delay of more than one year occur between the date of approval of the management scheme and either the date of implementation of the management scheme or the date of development commencing, a further site survey must be undertaken and submitted to the Local Planning Authority for approval. Any such scheme shall be implemented in full.

Reason - To prevent the spread of Japanese Knotweed, which has been found on part of the site in accordance with the Wildlife and Countryside Act 1981.

27) Notwithstanding the provisions of the Town and Country Planning (General Permitted Development) Order 2015 (or any order revoking and re-enacting that Order with or without modification) none of the dwellings hereby approved shall be used for any other purpose (including any other purpose in Class C3(a) of the Schedule to the Town and Country Planning (Use Classes) Order 1987 as amended by The Town and Country Planning (Use Classes) (Amendment) (England) Order

2010, or in any provision equivalent to that Class in any statutory instrument revoking and re-enacting that Order with or without modification) other than the purpose(s) of C3(a).

Reason - In the interests of residential amenity, to safeguard the character of the area and to maintain the sustainability of the local community through provision of accommodation that is suitable for people living as families pursuant to policies DM1 and H11 of the Core Strategy for Manchester and the guidance contained within the National Planning Policy Framework.

28) Prior to development commencing a local labour agreement shall be submitted to and agreed in writing with the City Council as local planning authority. The approved scheme shall be in place prior to the commencement of the development, and shall be kept in place thereafter.

Reason - To safeguard local employment opportunities, pursuant to policies EC5 of the Core Strategy for Manchester.

29) No development shall take place until the applicant or their agents or successors in title has secured the implementation of a programme of archaeological works. The works are to be undertaken in accordance with a Written Scheme of Investigation (WSI) submitted to and approved in writing by Manchester Planning Authority. The WSI shall cover the following: 1. A phased programme and methodology of investigation and recording to include: - an archaeological desk based assessment - informed by the above, archaeological evaluation through trial trenching - dependent on the above, targeted more detailed area excavation and recording 2. A programme for post investigation assessment to include: - production of a final report on the significance of the below-ground archaeological interest. 3. Deposition of the final report with the Greater Manchester Historic Environment Record. 4. A scheme for disseminating the history and archaeology of the site. 5. Provision for archive deposition of the report and records of the site investigation. 5. Nomination of a competent person or persons/organisation to undertake the works set out within the approved WSI.

Reason: In accordance with NPPF Section 12, Paragraph 141 - To record and advance understanding of heritage assets impacted on by the development and to make information about the heritage interest publicly accessible. GMAAS will monitor the implementation of the archaeological works on behalf of Manchester Planning Authority.

30) Notwithstanding information shown on plan ref: PR/17/ AH01/GA/01 Rev. D (landscape works), no landscaping works shall commence until there has been submitted to and approved in writing by the local planning authority a plan indicating the finalised positions, design, materials and type of boundary treatment to be erected on the site, together with the proposed timing of the erection of the boundary treatment. Development shall be carried out in accordance with the approved details and shall thereafter be retained.

Reason - To ensure that the appearance of the development is acceptable to the City Council as local planning authority in the interests of the visual amenity of the area

within which the site is located in order to comply with d policies SP1 and DM1 of the Core Strategy for Manchester.

31) Prior to the installation of the gates to the access to the cemetery courtyards referred to in the Crime Impact Statement finalised details of the location elevational design detailed specifications of the method of gate control (closure and locking mechanisms) shall be submitted to and approved in writing by the local planning authority. The scheme shall subsequently be implemented in accordance with the approved details before the development is completed.

Reason - In the interests of highway safety and pedestrian safety, pursuant to policies DM1, SP1 and T1 of the Core Strategy for Manchester.

32) No development shall take place until surface water drainage works have been implemented in accordance with Non-Statutory Technical Standards for Sustainable Drainage Systems (March 2015) or any subsequent replacements national standards and details that have been submitted to and approved in writing by the Local Planning Authority. The details to be submitted shall include :-

- Details of surface water attenuation that offers a reduction in surface water runoff rate in line with the Manchester Trafford and Salford Strategic Flood Risk Assessment, i.e. at least a 50% reduction in runoff rate compared to the existing rates, as the site is located within Conurbation Core Critical Drainage Area. Existing brownfield runoff rates should be based on the capacity of the existing drainage system;
- Evidence that the drainage system has been designed (unless an area is designated to hold and/or convey water as part of the design) so that flooding does not occur during a 1 in 100 year rainfall event in any part of a building;
- Assessment of overland flow routes for extreme events that is diverted away from buildings (including basements). Overland flow routes need to be designed to convey the flood water in a safe manner in the event of a blockage or exceedance of the proposed drainage system capacity. The flood water should be routed away from the buildings and towards the less vulnerable areas i.e. open spaces, car parks and roads. A layout with overland flow routes needs to be presented.
- A 40% uplift to account for climate change impact should be considered in line with revised climate change allowances published by the Environment Agency in 2016;
- Hydraulic calculation of the proposed drainage system;
- Construction details of flow control and SuDS elements.

Reason: To promote sustainable development, secure proper drainage and to manage the risk of flooding and pollution pursuant to national policies within the NPPF and NPPG, and policies EN8, EN14 and DM1 of the Core Strategy for Manchester.

33) No development hereby permitted shall be occupied until details of the implementation, maintenance and management of the sustainable drainage scheme have been submitted to and approved by the local planning authority. The scheme shall be implemented and thereafter managed and maintained in accordance with the approved details. Those details shall include:

- Verification report providing photographic evidence of construction as per design drawings;
- As built construction drawings if different from design construction drawings;
- Management and maintenance plan for the lifetime of the development which shall include the arrangements for adoption by any public body or statutory undertaker, or any other arrangements to secure the operation of the sustainable drainage scheme throughout its lifetime.

Reason - To ensure a satisfactory method of surface water disposal and prevent the increased risk of flooding elsewhere, and ensure future maintenance of the surface water drainage system, pursuant to policies EN8, EN14 and DM1 of the Core Strategy for Manchester.

Local Government (Access to Information) Act 1985

The documents referred to in the course of this report are either contained in the file(s) relating to application ref: 115139/FO/2017 held by planning or are City Council planning policies, the Unitary Development Plan for the City of Manchester, national planning guidance documents, or relevant decisions on other applications or appeals, copies of which are held by the Planning Division.

The following residents, businesses and other third parties in the area were consulted/notified on the application:

Highway Services
Contaminated Land Section
Environmental Health
Strategic Development Team
Greater Manchester Police
Transport For Greater Manchester
Greater Manchester Archaeological Advisory Service
Greater Manchester Ecology Unit
Contaminated Land Section
Environmental Health
Highway Services
Strategic Development Team
Greater Manchester Ecology Unit
Greater Manchester Archaeological Advisory Service
Greater Manchester Police
Transport For Greater Manchester

A map showing the neighbours notified of the application is attached at the end of the report.

Representations were received from the following third parties:

26 Greenhill Road on behalf of 21 Saddlecote Close, Crumpsall, Manchester, M8 5QE

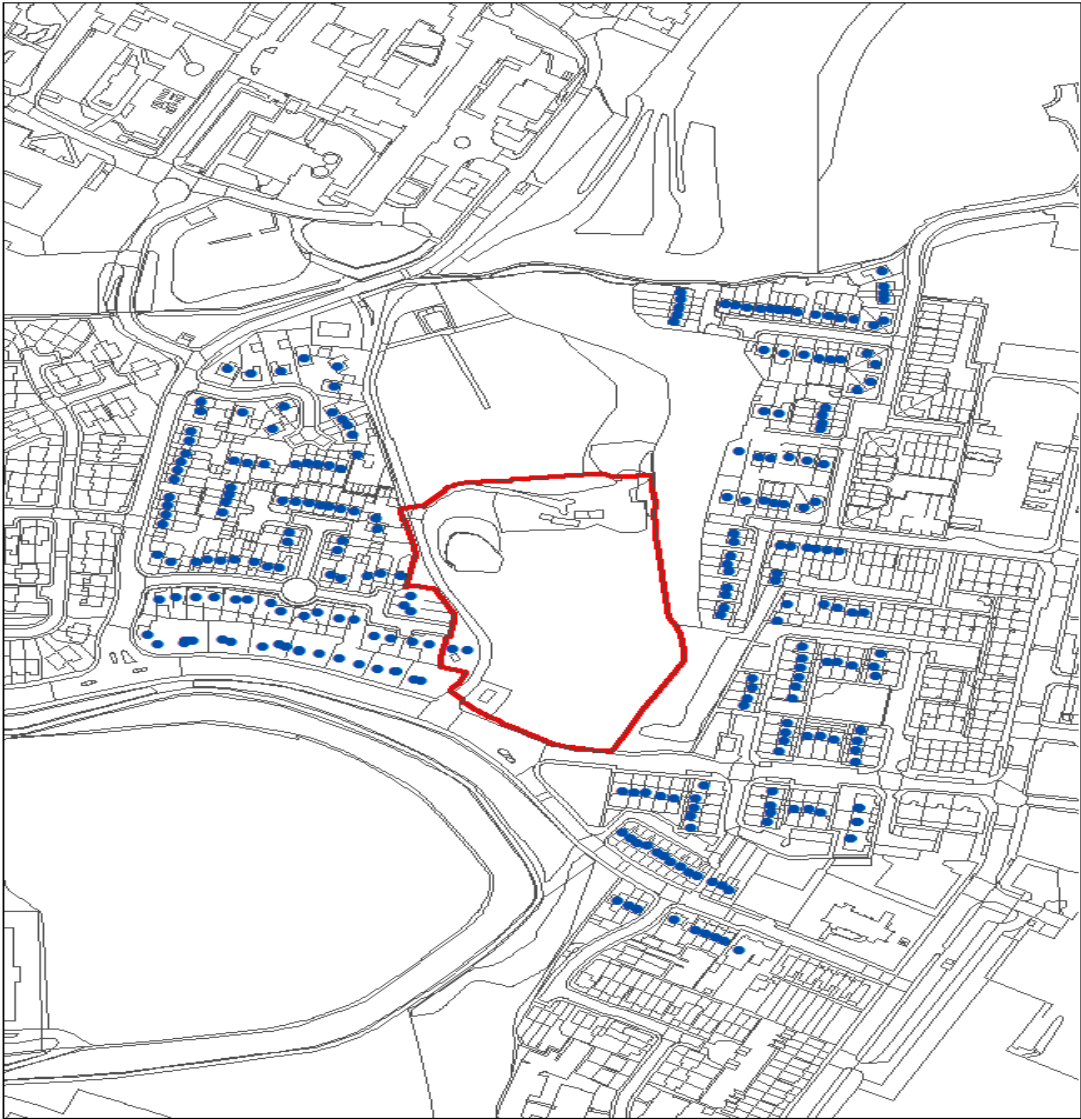
49 Saddlecote close, crumpsall, manchester, M8 5QE

51 Saddlecote close, crumpsall, manchester, M8 5QE

65 Saddlecote close, crumpsall, manchester, M8 5QE

Saddlecote close, crumpsall, Manchester.

Relevant Contact Officer : Sue Wills
Telephone number : 0161 234 4524
Email : s.wills@manchester.gov.uk



 Application site boundary  Neighbour notification
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